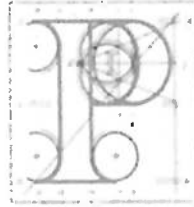


Our Case Number: ABP-322638-25



**An
Coimisiún
Pleanála**

Iarnód Éireann
Third Party Approvals Office
Pearse Station
Westland Row
Dublin 2

Date: 12 August 2025

Re: Proposed mixed use residential development of 1,250 no dwelling units, creches, community hall, retail and assoc. site works including Grange House at the Kishoge Development area of Clonburris SDZ In the townlands of Kishoge, Esker South, Grange and Balgaddy, Clonburris, County Dublin

Dear Sir / Madam,

An Coimisiún Pleanála has received your recent submission in relation to the above mentioned proposed development and will take it into consideration in its determination of the matter.

Please note that the proposed development shall not be carried out unless the Commission has approved it or approved it with conditions.

If you have any queries in relation to the matter please do not hesitate to contact the undersigned officer of the Commission at laps@pleanala.ie

Please quote the above mentioned An Coimisiún Pleanála reference number in any correspondence or telephone contact with the Commission.

Yours faithfully,

Lauren Griffin
Executive Officer
Direct Line: 01-8737244

JA03

Teil
Glaó Áitiúil
Facs
Láithreán Gréasáin
Ríomhphost

Tel
LoCall
Fax
Website
Email

(01) 858 8100
1800 275 175
(01) 872 2684
www.pleanala.ie
communications@pleanala.ie

64 Sráid Maoilbhríde
Baile Átha Cliath 1
D01 V902

64 Marlborough Street
Dublin 1
D01 V902



An Bord Pleanála
64 Marlborough Street
Rotunda
Dublin 1
D01 V902

Date: 11th July 2025.
Planning Application, No: JA06S.322638.

Development: Proposed residential development at the Kishoge Development area of Clonburris SDZ.

Location: In the townlands of Kishoge, Esker South, Grange and Balgaddy, Clonburris, County Dublin.

Line: Heuston to Cork

Mileage: 06-00

AN COIMISIÚN PLEANÁLA	
LDG-	_____
ACP-	_____
11 4 JUL 2025	
Fee: €	_____ Type: _____
Time: 9/10	By: <u>RDG P2752</u> PM

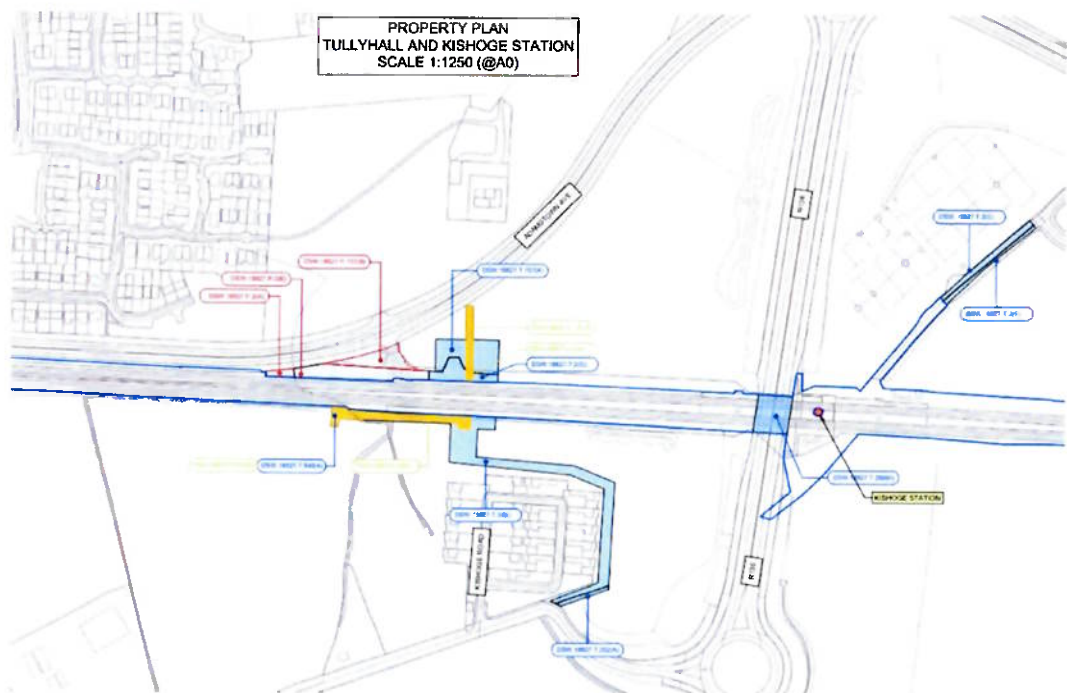
Iarnród Éireann Recommendation: **Observations only**

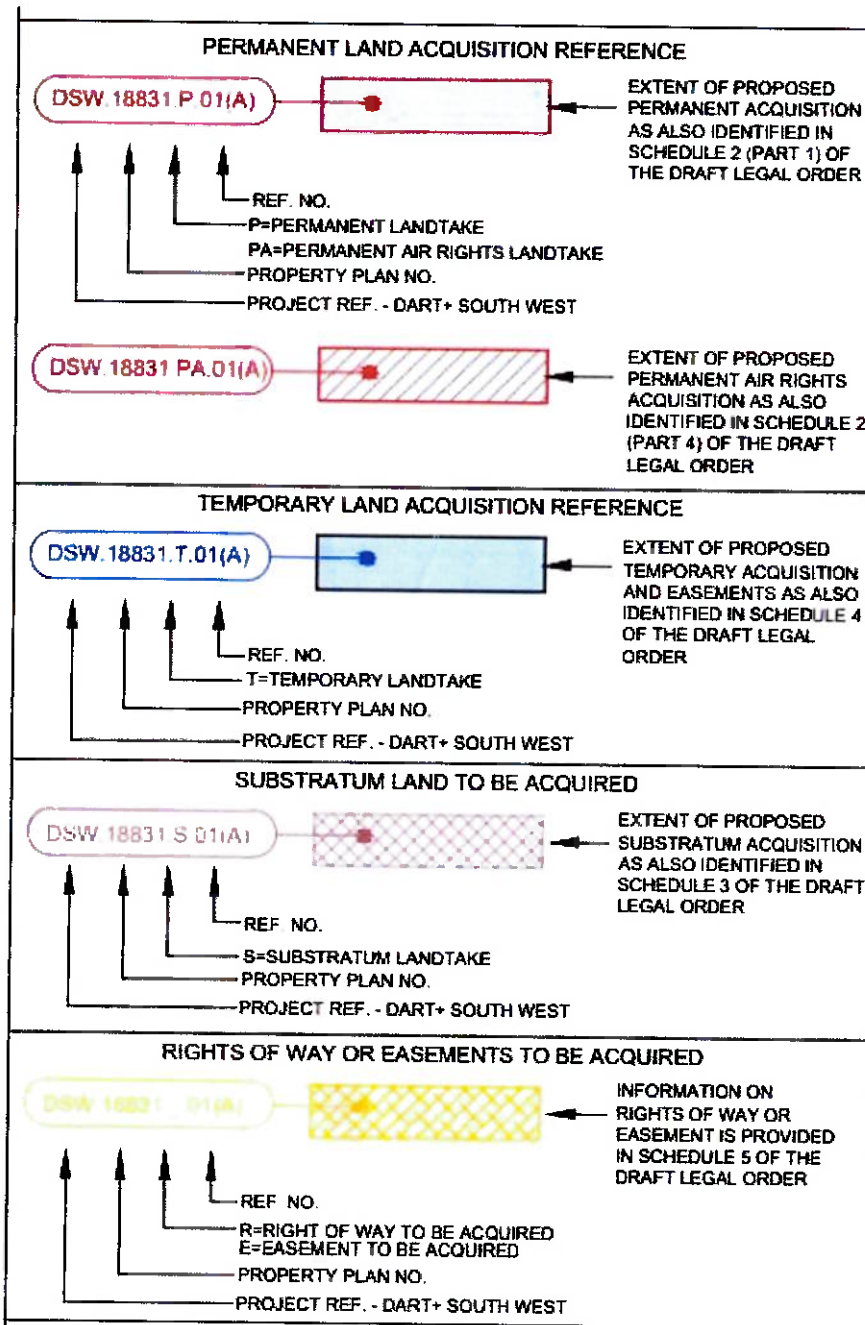
Iarnród Éireann has the following **observations** in respect of the proposed development:

- 1) The Railway Safety Act 2005 places an obligation on any 3rd party working near the railway to ensure no danger or hazard is posed to railway operations. Due to the proximity of this site to the railway corridor, the Applicant must take due consideration of this obligation during the planning and construction of the development.
- 2) Given the close proximity of the proposed Residential Development to the DART+ South West Project, it is essential that coordination takes place between Iarnród Éireann, C.I.É, the DART+ South West Project Team, and the Applicant. This is due to the potential impact of the development on lands identified for both temporary and permanent acquisition, as well as rights of way and easements to be acquired for the delivery of the DART+ South West project.

The property plans for the DART+ South West Project, which are included below for reference, clearly outline areas subject to permanent and temporary land acquisition, are critical for the construction and long-term operation of the rail infrastructure. A preliminary review of the submitted boundary for the proposed development suggests there may be overlaps or conflicts with these designated lands.

In light of this, early engagement and agreement between all relevant parties is strongly advised to ensure that the proposed development does not interfere with the timely delivery or future operation of the DART+ South West Project. Access to the full set of property plans can also be found at the following link: <https://www.dartplus.ie/getmedia/7356a0e5-234a-4981-b848-d1214cd15fa4/Book-2-Property-Plans.pdf?ext=.pdf>





- 3) Due to proximity of the Heuston to Cork railway line, a 2.4m high suitable designed, solid block boundary treatment, should be erected by the applicant on the applicants' side of the property boundary. The maintenance of this boundary treatment rests with the Applicant and the Successor-in-Title. The exact location and details of this boundary treatment must be identified on site in co-operation with this office.
- 4) The security of the railway boundary must be maintained at all times during construction where new boundary treatment is to be installed. Preferably the permanent boundary treatment should be completed early in the construction programme.
- 5) At no point should the Applicant or their representatives enter railway property to undertake the construction of this development. It is imperative that the railway always remains inaccessible to prevent trespassing, both during the construction phase and in the future.

This includes safeguarding against unauthorized access, especially through scaffolding or any other means.

- 6) The boundary of the proposed development lies directly adjacent the Heuston to Cork Railway line. The site boundary shown on the plan drawings assumes that the property boundary to railway lands corresponds with OSi mapping or existing fence lines. This approach is unreliable and may not reflect the full extent of railway lands acquired to construct, maintain and operate the railway. It is usually the case that drainage channels running parallel to the railway at the crest of cuttings and the toe of embankments fall within the original land take for the railway corridor. It is therefore possible that the proposed development encroaches onto Iarnród Éireann lands. The Applicant should engage with the Third-Party Co-ordinator by the email 3rdpartyapprovals@irishrail.ie and provide detailed cross sections with a view to agreeing the line of the proposed boundary.
- 7) C.I.É owns a section of Lynch's Lane, which historically formed part of a former railway crossing. This lane currently provides essential access to a gate leading to the existing signalling room and other critical railway infrastructure. Iarnród Éireann requests that construction activities ensure this access route is maintained and kept clear at all times, both throughout the construction phase and following project completion, to guarantee uninterrupted operational access. Permanent, long-term access to the railway infrastructure via Lynch's Lane must be preserved.

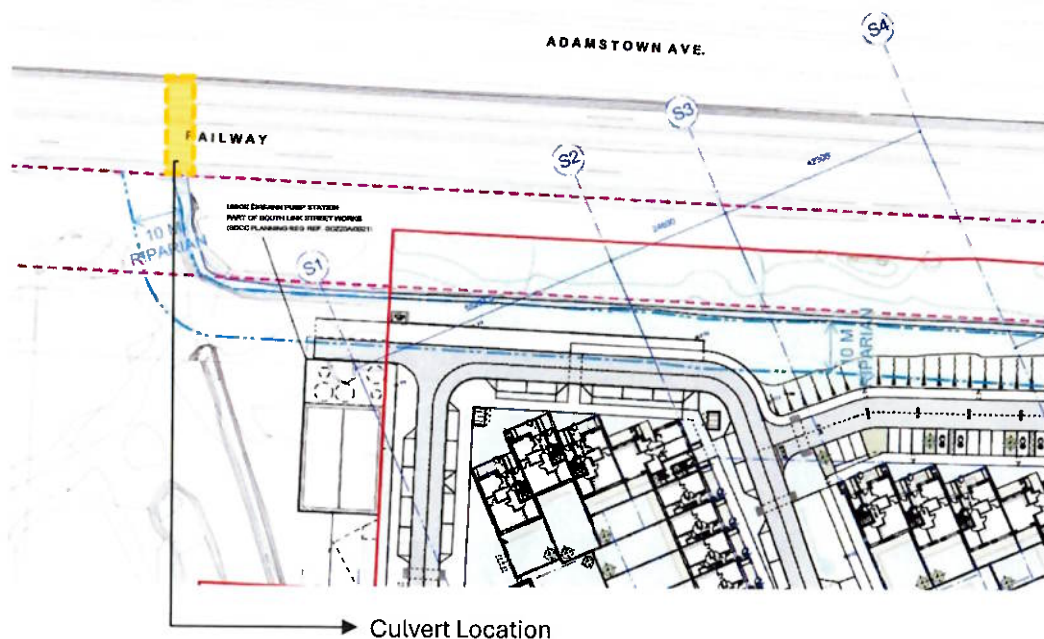


Access Gate Location

- 8) No additional liquid, either surface water or effluent shall be discharged to, or allow to seep onto, the railway property or into railway drains / ditches. Iarnród Éireann seeks clarification regarding the area near the boundary identified as a flood compensation storage area. Further details are required to confirm that this proposed feature will not impact railway land or infrastructure, either directly or indirectly.



- 9) The Applicant should note the presence of the railway culvert UBC14A which coincides with this development site. Applicant must take account of the requirements for railway staff to be able to access the portal of the culvert for the purposes of regular inspection and maintenance where necessary.



- 10) Lights from the proposed development, either during the construction phase or when the development is completed, should not cause glare or in any way impair the vision of train drivers or personnel operating on track machines.
- 11) There shall be a horizontal separation distance of 5 meters minimum between the two proposed carrier sleeves and/or any existing undertrack pipe or duct (taken from outer face of proposed sleeve to the nearest outer face of existing undertrack pipe or duct).
- 12) Should the development require the use of a crane that could swing over the railway property, then the developer must enter into an agreement with Iarnród Éireann / CIÉ. regarding this issue.

- 13) Any proposed services that are required to cross along, over or under the railway property must be the subject of a license agreement with Iarnród Éireann / C.I.É. Please note that, although C.I.É grants landlord's consent to apply for permission, the proposal shall be subject to the approval of the Board of C.I.É.
- 14) No overhang of any part of the development over the railway property is to be allowed.
- 15) No deciduous trees are to be planted directly along the railway boundary as they can impair the vision of train drivers or their views of signals, etc. Furthermore, falling leaves and / or leaf litter on rails can adversely affect the operation of trains by causing poor wheel / rail adhesion.
- 16) Should you intend to cut down trees that are in proximity of the railway line such that if they were to fall towards the line, they would block it, the Applicant must contact the Third-Party Co-ordinator by the email 3rdpartyapprovals@irishrail.ie for a safe system of work to be established to undertake this work.
- 17) Due to the size and nature of the proposed project The Applicant and / or his Agents should make direct Contact with The Third-Party Co-ordinator by the email 3rdpartyapprovals@irishrail.ie, to discuss this application and its impact on the railway in more detail.
- 18) The railway operates 24 hours a day with maintenance activity taking place at night and during shutdowns of passenger services. The development is in close proximity to the live railway and therefore must take account of the potential noise and vibration impact that an operational railway may have on sensitive receptors. Residential units should be designed, orientated and located to limit the impacts of noise and vibration from transportation traffic and maintenance activities. It is recommended that the Applicant incorporates best practice principles in the design using BS8233 - Guidance on Sound Insulation and Noise Reduction for Buildings. The Applicant must refer to the local authority's Noise Action Plan regarding development adjacent railways and where appropriate carry out a noise risk assessment to inform an Acoustic Design Statement (ADS). The detail of the ADS should be commensurate with the level of risk identified in the noise risk assessment. The Applicant must take responsibility for specifying necessary mitigation measures where noise thresholds are expected to be exceeded. The noise assessment should consider a number of scenarios, including the following:
 - within the development with windows closed.
 - within the development with windows open; and
 - exterior of development within private or communal gardens.

These observations prioritize railway operational safety, and we advise that these points are communicated to the Applicant. Furthermore, these considerations should be incorporated as a condition of planning approval if the application is successful.

I.É Third Party Approvals Office

APPENDIX A

An Bord Pleanála
64 Marlborough Street
Rotunda
Dublin 1
D01 V902

Date: 11 July 2025

Re: Proposed residential development at the Kishoge Development area of Clonburris SDZ (Planning Application, No: JA06S.322638.)

Dear Sir/Madam,

On behalf of DART+ South West Project, we would like to make the following observations in relation to the above planning application.

The proposed housing development is adjacent to and overlaps with the DART+ South West project works and/or land boundaries (permanent and temporary acquisition & easements).

Our submission is structured as follows:

1. Background – DART+ South West project
2. Review of Housing Proposal Application - "Site 3"
3. Review of Housing Proposal Application - "Site 5"

1. Background – DART+ South West project

The DART+ South West project will deliver an improved electrified network, with increased passenger capacity and enhanced train service between Hazelhatch & Celbridge Station to Heuston Station (circa 16km) on the Cork Mainline, and Heuston Station to Glasnevin via the Phoenix Park Tunnel Branch Line (circa 4km). The key infrastructural elements of the DART+ South West project include:

- Completion of four tracking from Park West & Cherry Orchard Station to Heuston Station.
- Electrification and re-signalling of the line from Hazelhatch & Celbridge Station to Heuston Station and also from Heuston Station to Glasnevin, via the Phoenix Park Tunnel Branch line, where it will link with DART+ West.
- Undertaking improvements/reconstruction of bridges to facilitate movement of electrified train services.
- Remove rail constraints along the Phoenix Park Tunnel Branch line; and
- Delivery of a Heuston West Station.

The DART+ South West project provides the transport capacity that underpins the new community developments in Adamstown and Clonburris.

Having made the Railway Order application in March 2023 (ABP Reference 316119-23), Iarnród Éireann/CIÉ received approval for the Railway Order on 13th November 2024.

We have started the process of completing contract documents and will shortly seek tenders for a main contractor.

2. Review of Housing Proposal Application - "Site 3"

A. Land acquisition for DART+ South West

The proposed development of SDCC "Site 3" overlaps the approved DART+ South West permanent land acquisition, temporary land acquisition and right of way or easement.

i. Permanent Land Acquisition for the Substation Compound

- DSW.18827.P.157(B) - In ownership of SDCC and Shelborne Development Ltd.
- DSW.18827.P.2(B) - In ownership of SDCC
- DSW.18827.P.2(A) - In ownership of SDCC

ii. Temporary Land Acquisition for Substation Works Compound and Utilities diversions.

- DSW.18827.T.2(T) - In ownership of SDCC
- DSW.18827.T.157(A) - In ownership of SDCC and Shelborne Development Ltd.

iii. Rights of Way or Easements for undergrounded utilities

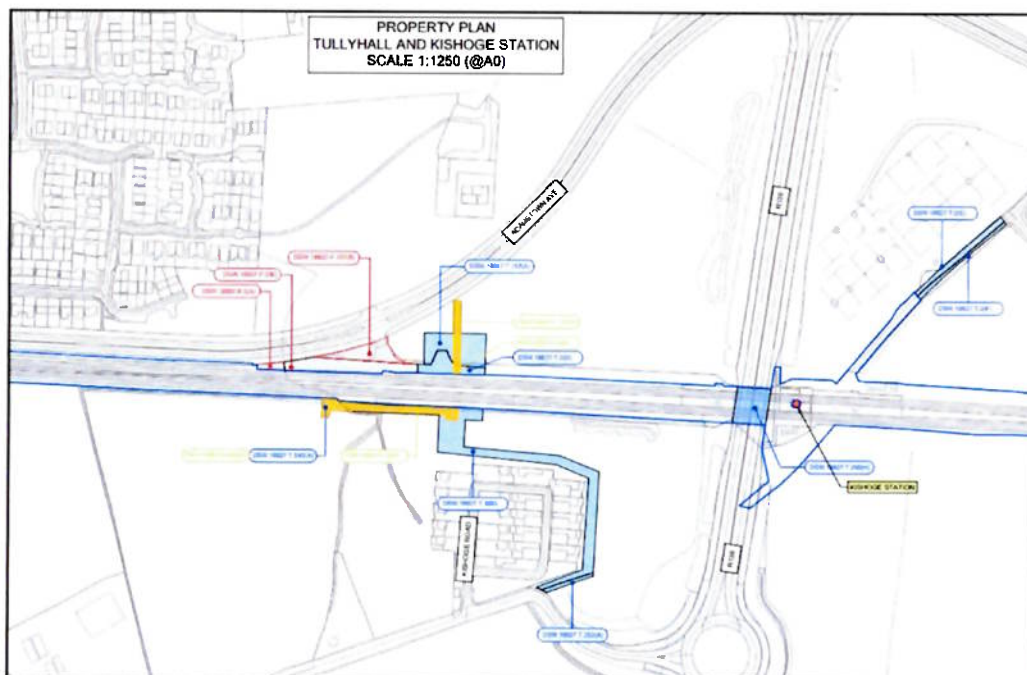
- DSW.18827.E.2(H) - In ownership of SDCC
- DSW.18827.E.157(E) - In ownership of SDCC and Shelborne Development Ltd.

The overall duration of construction will be approximately 5 years. The detailed construction programme will only be completed upon the appointment of a main contractor, at this point we do not know when precisely these works will take place.

The housing development must take these CPO requirements into consideration.

The housing development proposal as presented, does not provide sufficient space to accommodate the utility diversion works approved by the Railway Order.

As it stands, the temporary land acquisition and right of way CPO in the approved Railway Order gives Iarnród Éireann exclusive rights for use of the land for said purposes.



Excerpt from Railway Order Property Plans, DART+ South West

B. DART+ South West Works in the SDCC "SITE 3":

The DART+ South West Works and CPO Plans along with the associated CPO schedules of the approved railway order indicate the following:

i. Permanent Land acquisition

- is required for a 38kV electrical traction substation; to enable electrification of the 2 No. northern DART lines. The substation will be located within a dedicated compound. A permanent access will be provided from existing Adamstown Avenue; using what is currently a bell-mouth entrance to the lands.

ii. Temporary Land acquisition

- is required for the diversion of 2no. existing 10kV track crossings; at approx. chainages 18+000 and 18+110. This land includes for the access route to the launch and receiving pits, the diversion route and for substation works compound. The compound will facilitate the undertrack crossing diversions and the ducting for a future 38kV crossing for ESB to a cable route to the substation from "Site 1" and through "Site 3".

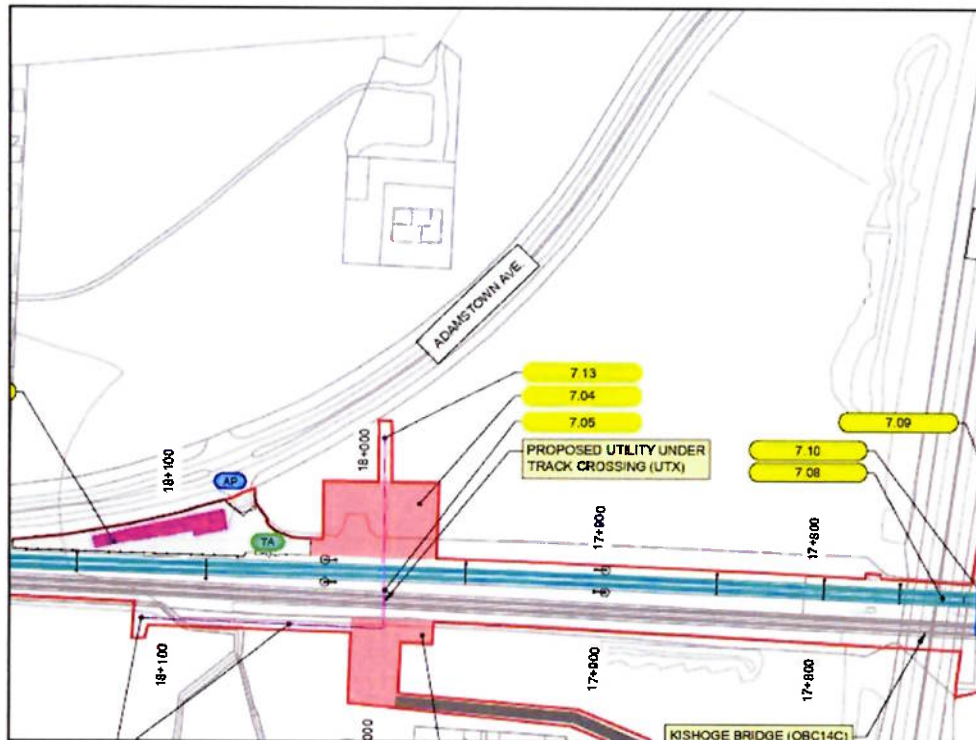
iii. The description of the routing of the 2no. MV cable diversions are as follows:

- At CH18+110 the existing undertrack crossing is proposed to be diverted from existing pole to south of railway in an eastward direction and along the southern railway corridor boundary (within the "SITE 1" development) to the proposed new CH17+995 railway crossing duct bank crossing. This would then head north up through "SITE 3" (the development under review) to connect to the same supply route (located within the development lands, as indicated in the existing ESB cable plans provided to Irish Rail).
- The 2nd diversion is the undergrounding of the existing overhead crossing at CH18+000 which was proposed to be undergrounded immediately adjacent to the overhead crossing at approx. CH17+995 also using horizontal directional drilling and also and connected on either side to the existing overhead networks using a section of underground cable and pole installation.
- The temporary land acquisition is also to account for installing the 38kv ducts across the railway corridor; required for a future ESB power supply to the substation. The early installation of ducting will serve to limit future impact on the developments which would require the potential demolition of road for launch and receiving pits.

iv. Cable and ROW easements acquisition

- are required for the above-mentioned cable routes were requested by ESB for any ESB undergrounding diversion routes.

All of the above said works are shown in the image below.



Excerpt from Railway Order Works Plans, DART+ South West

C. Iarnród Éireann concerns and Recommended Conditions

Having reviewed the Part 10 of the Planning application documentation the following observations are made:

i. Space required for 38kV and MV HDD works

The launch and receiving pits, associated equipment and the existing overhead lines comprise a substantial portion of the DART+ South West compound.

ii. Development building located on top of the DART+SW land acquisition

for cable crossing and substation construction compounds; as well as parts of the cable easement for cable routing. Should this be ignored we will be unable to connect the traction substation planned to be adjacent to your development.

iii. Proposed watermain clashes with the approved acquisition

The proposed watermain crossing underneath the railway corridor, running between Adamstown Avenue and SDCC "SITE 1" (through SDCC "SITE 3"), clashes with the approved acquisition in DART+SW Railway Order operative since November 2024. Should this land acquisition be ignored the DART+ South West project will be unable to connect the traction substation adjacent to your development.

- The watermain design needs to be agreed with ESB, who must confirm acceptance of the design such that it does not compromise the routing of the 38kV cable power supply route to the Kishoge traction substation and and/or the proposed MV cable diversion routes (including their undertrack crossings).
- Note that the 38kV, watermain and MV crossings are to cross the railway track at a depth that provides 4.5m clearance to the utility.

iv. Proposed Telecoms Mast proximity to Substation Compound

The proposed Telecommunications mast compound, located by the developer at the eastern end of the development is shown to be adjacent to the DART+ proposed substation compound.

- The fencing and/or foundations of fence and mast should be located a minimum of 1m from the CPO permanent acquisition line.
- The substation compound will have a 3.6m high palisade fence that will need to be installed and maintained; accordingly the and it is not desirable to have the fences back to back.

3. Review of Housing Proposal Application - Site 5

The proposed housing development is adjacent to and overlaps with the DART+ South West approved project. We are particularly concerned in relation to construction stage access to the DART+ South West project and how this development might conflict with that.

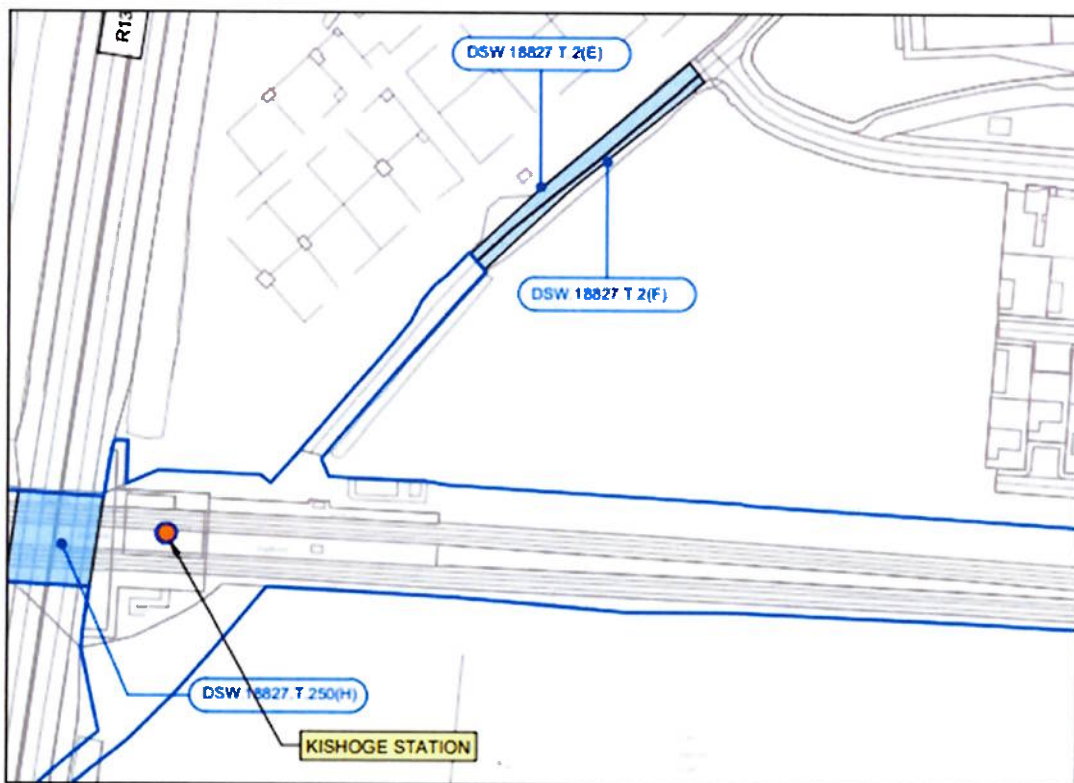
A. Land acquisition for DART+ South West

A temporary land acquisition is included along the access lane to the rail line in the vicinity of the proposed housing scheme. This is to ensure that access to the DART+ South West construction site, and to the railway track, is enabled during the construction period. Without track access, construction of the DART+ cannot proceed.

i. Two Separate Temporary Acquisition Plots cover the access road:

- 18827.T.2(E) – 464.9 sq. m – Ownership of SDCC – Lynch Lane, Kishoge
- 18827.T.2(F) – 269 sq. – Ownership of SDCC – Lynch Lane, Kishoge

The remainder of the lane is in the ownership of CIÉ.



Excerpt from Railway Order Property Plans, DART+ South West

The overall duration of construction will be approximately 5 years. The detailed construction programme will only be completed upon the appointment of a main contractor, at this point we do not know when precisely these works will take place.

The housing development must take these CPO requirements into consideration.

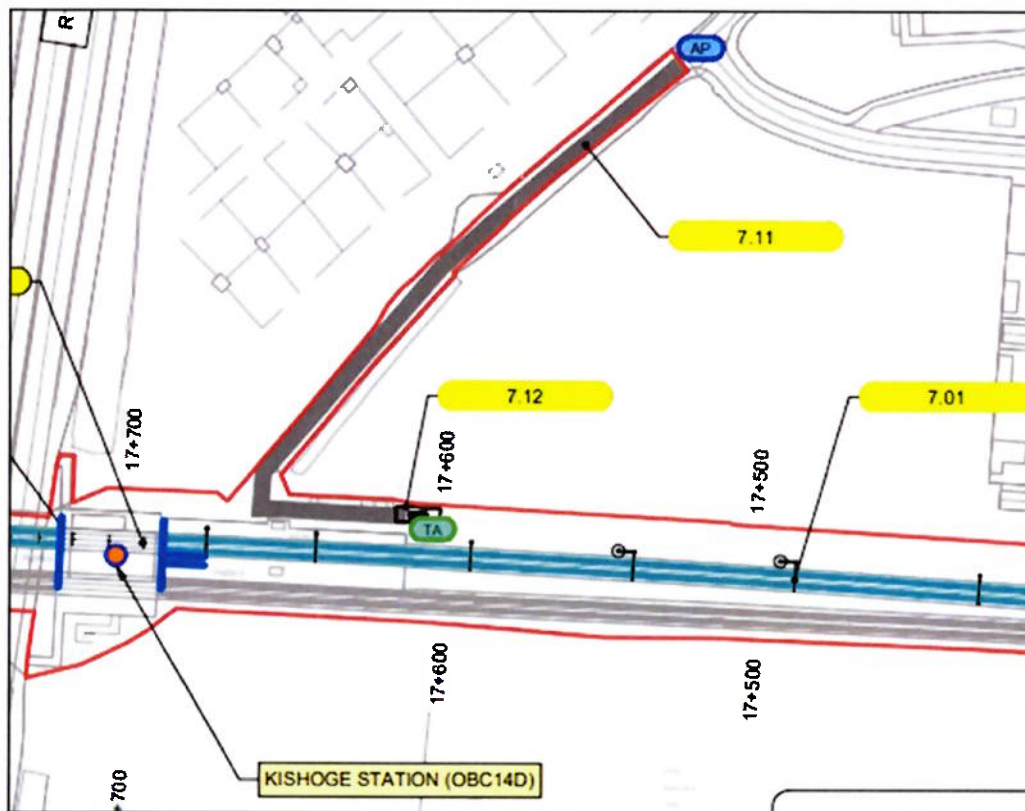
The housing development proposal as presented, does not provide sufficient space to accommodate the utility diversion works approved by the Railway Order.

As it stands, the temporary land acquisition CPO in the approved Railway Order gives Iarnród Éireann exclusive rights for use of the land for said purposes.

B. Existing Track Access Point and other Works at this location.

As shown on the DART+ South West Works Plan, there is an existing CIÉ track access at this point. The draft Railway Order includes the following:

- The approved DART+ South West Railway Order indicates an existing permanent track access point (TA) at the end of the access road to the railway corridor; both the access road and track access point are to be retained for future operational and maintenance requirements of the electrified railway.
- The Railway Order enables IÉ to re-gravel and re-grade the existing access lane, as necessary, to access the rail line during the construction period (Works No. 7.11)



Excerpt from Railway Works Plan, DART+ South West

C. Iarnród Éireann concerns and Recommended Conditions

Having reviewed the Part 10 Planning application documentation,

i. Access to construct the DART+ South West project:

Construction of the DART+ South West project requires access to the rail line at this location, in addition to its maintenance and operational function purposes. This needs to be guaranteed notwithstanding housing construction requirements and phasing; and should be sufficient for articulated long wheel base flatbed trucks to use.

The access road connectivity interface between the private road (along which CIE has a right of way) and the road within CIE lands; seems to be severed by a road bend in the proposed development. This connectivity must be uninterrupted, details of the same should be provided and agreed with IE (at grade connection on the road bend, or dished

driveway entrance, SDCC may not accept the modification of a bend to a T-junction junction).

ii. Layout looks to encroach into CIE lands:

The site layout indicates a future connection road over CIÉ lands that will form part of the scheme. and also seems to encroach onto CIE Lands. Developer to confirm that no development on CIÉ land is proposed as part of the application.

iii. Boundary with Lane:

There is no detail presented on the proposed boundary treatment along the access lane and at the interface of the CIÉ and SDCC lands at Lynch Lane. An appropriate boundary treatment conforming to Iarnród Éireann Standards should be included along the CIÉ lands on Lynch's Lane. This should include for vehicular gates and/or integrated pedestrian gate as will be stipulated by Irish Rail at detailed design.

Yours sincerely



Ricardo Santos

Programme Manager, DART+ South West, Iarnród Éireann

LINKS to DART+ South West Documentation

[DART+ South West Railway Order Application \(dartplus.ie\)](http://dartplus.ie)

[DART+ South West \(dartplus.ie\)](http://dartplus.ie)